

ASHE Altoona/PennDOT 9-0



20th ANNUAL JOINT WORKSHOP

Monday, April 18, 2022



CENTRAL OFFICE ORG CHANGE

- **Chief Executive: Jon Fleming**
 - Chief Engineer: Gavin Gray
 - Bureau of Maintenance
 - Vacant
 - Safety, Fleet, Tech Leadership, Asset Mgmt
 - Bureau of Operations
 - Vacant
 - OPO, Maint Support, Program Analysis/Metrics
 - Bridge Office
 - Vacant
 - Now includes Inspection
 - Bureau of Construction & Materials
 - Acting
 - Bureau of Design & Delivery
 - C. Spangler



DESIGN DIVISION

PennDOT District 9-0 Design



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Assistant District Executive
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Ashey Conese
Clerk Typist 2
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Updated 03/14/2022

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Dstr Util Adm
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Utility Relocation Technician
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Sr. Civil Engineer Supervisor
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Geospatial Specialist
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Envtl Plnr 2
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Andy Gonsman
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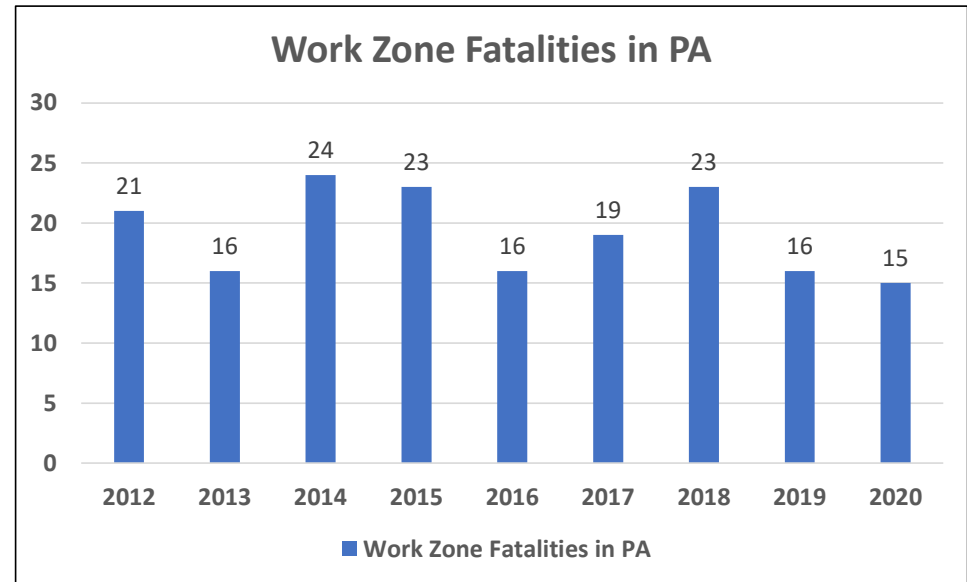
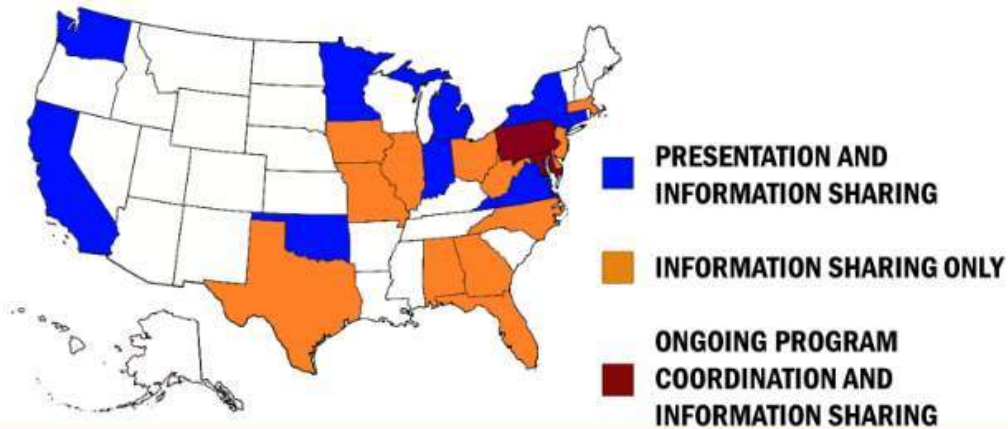
Safety



AUTOMATED WORK ZONE SPEED ENFORCEMENT

➤ AWZSE

PROGRAM OUTREACH – PEER-TO-PEER MEETINGS



RESULTS:
Excessive Speeds (>11 mph)
~10% April 2020
<3% June 2021



Automated Work Zone Enforcement Deployments - 2021





















Deployed on a total of five District 9 projects:

- I-99 Turnpike to Sproul
- US 219 McNally Bridge
- US 22 Cambria Interchanges
- US 219 Meyersdale Bypass
- US 219 Misc Maintenance Work

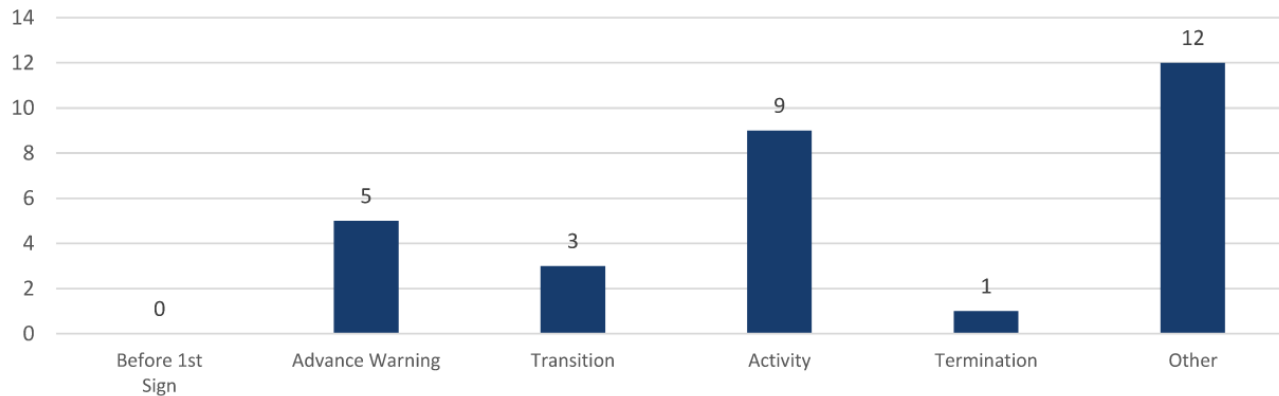
DISTRICT 9 AWZSE DEPLOYMENTS SUMMARY								
	Number of Deployments	Potential Violations	Violations per Deployment	Violations Issued	Percent over Speed Limit	Percent Excessively Speeding*	Average Excessive Speed	MPH over Threshold Speed
I-99 Section 013	37	216	5.84	182	11.62%	0.24%	73.31	2.31
US 219 Section 46B	57	307	5.39	266	6.23%	0.22%	78.27	2.32
US 22 Section 029	81	57	0.70	46	0.66%	0.02%	79.89	3.89
US 219 Section 003	79	235	2.97	173	10.27%	0.87%	68.47	2.47
US 219 Maintenance	6	1	0.17	1	1.60%	0.02%	82.00	6.00
Total	260	816	3	668	6.07%	0.27%	76.39	3.40

*Vehicles traveling at or over the threshold speed of 11 mph or over the posted speed limit

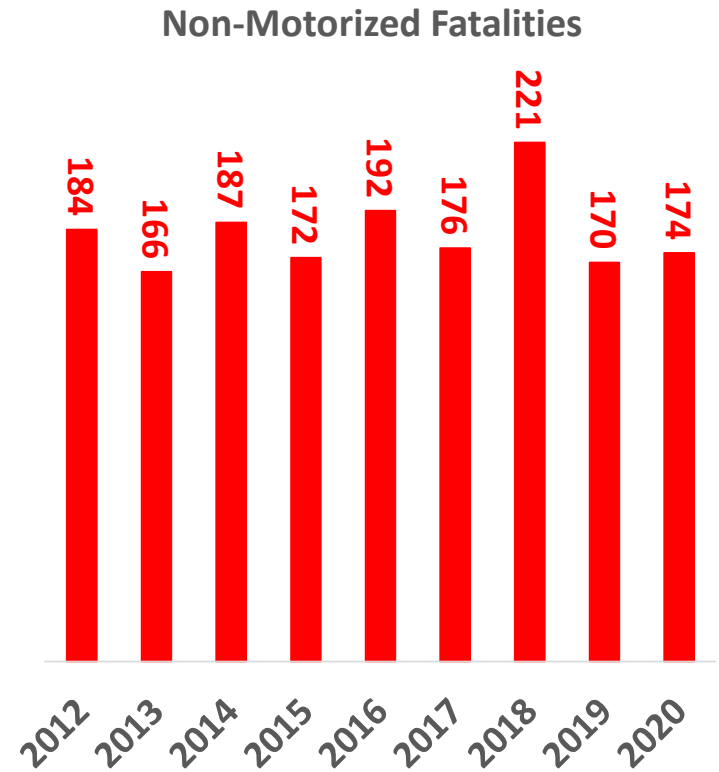
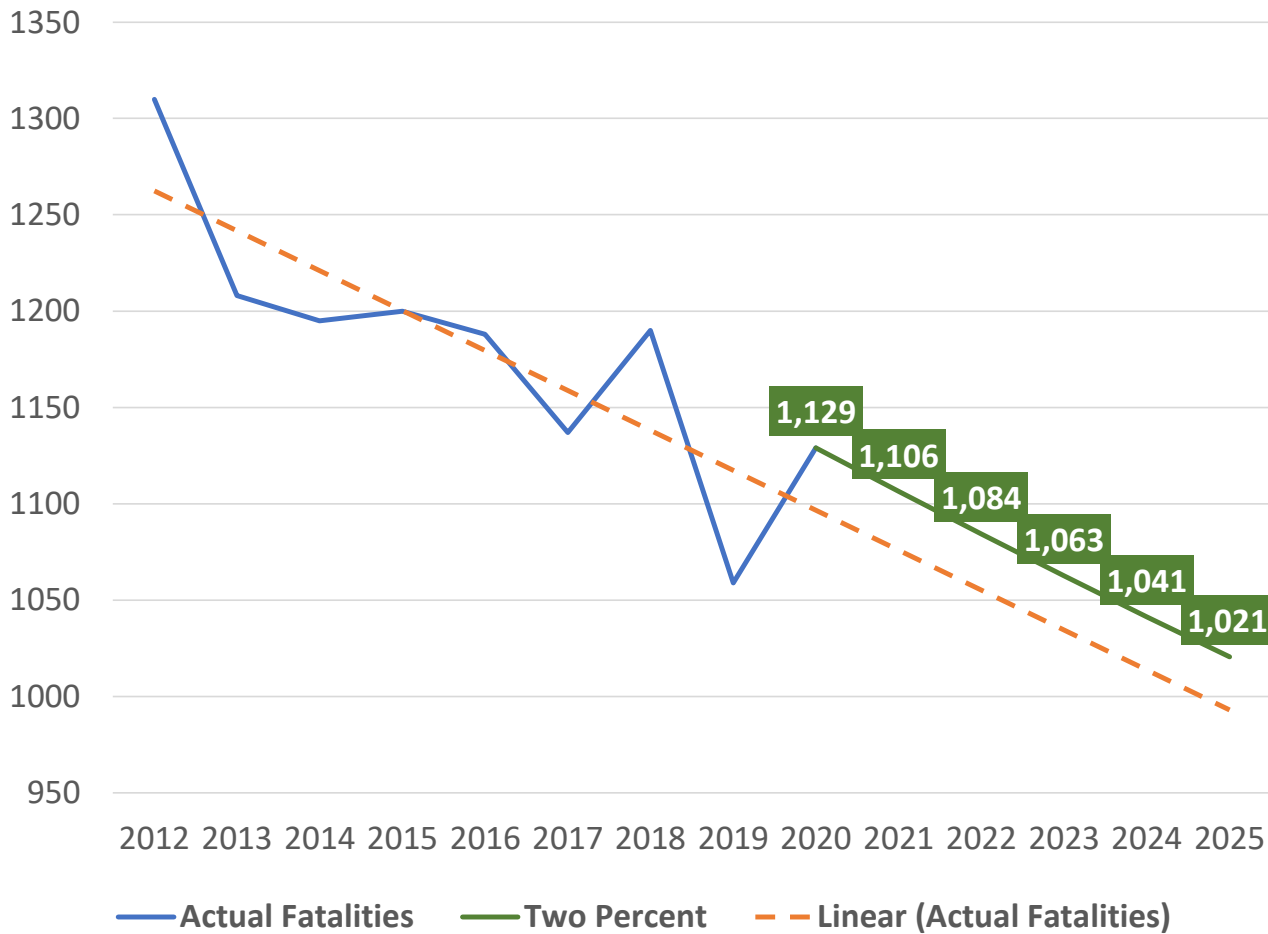
District TTCZ Performance Summary

D9-0				2020				Trend from '19				State Avg.			
Safety Metrics				Mobility Metrics				Management Metrics							
TTCZ Crashes per Year		30		129	Peak Delay (hr.)		63K		328K	Traffic Control QA/QC		92		90	
Rear-End Crashes		15		50	Non-Peak Delay (hr.)		322K		1,553K	Work Zone Intrusions		10		7.7	
Veh. Crash Speed over WZSL		32%		28%	% Time Queue >1mi.		26%		22%	PSP Assist.		\$65K		\$387K	
					TTR: Planning Time Index		1.16		1.20 <small>(on-time arrival TT multiplier)</small>						

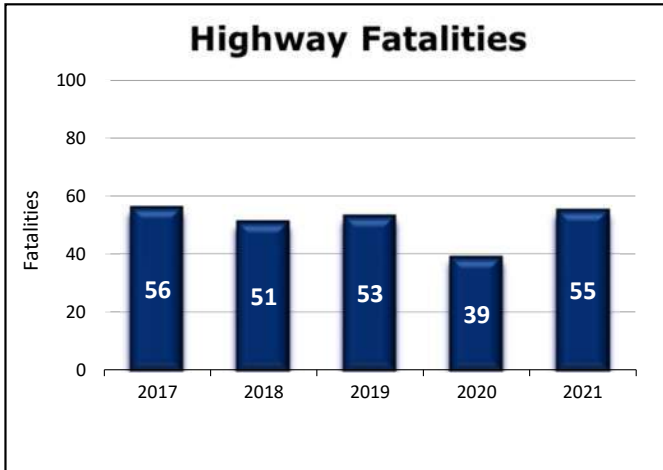
TTCZ Crash Location



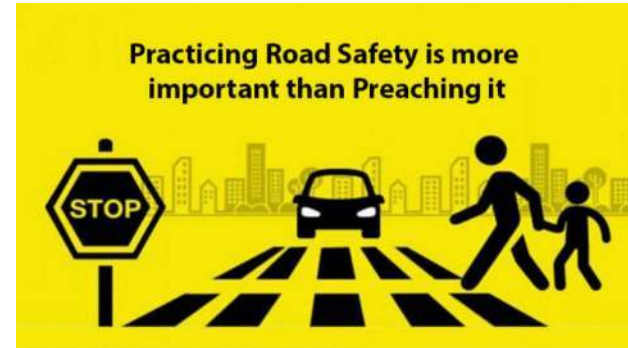
FATALITIES



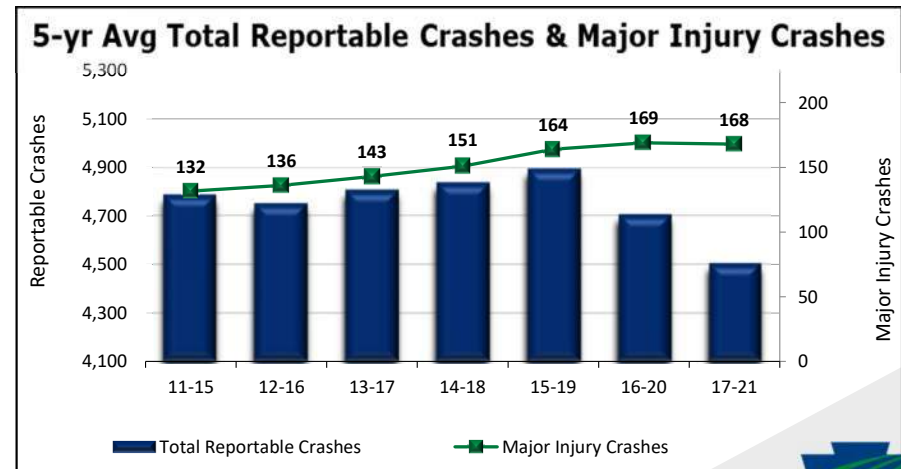
SAFETY




District 9



Beginning 2016 injury definitions changed to align with Model Minimum Crash Criteria 4th Edition

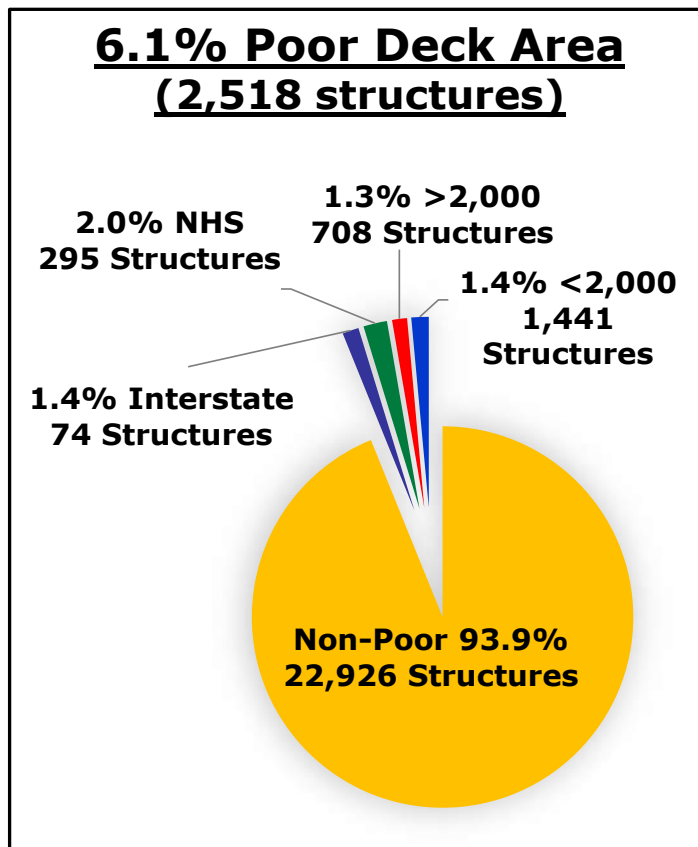




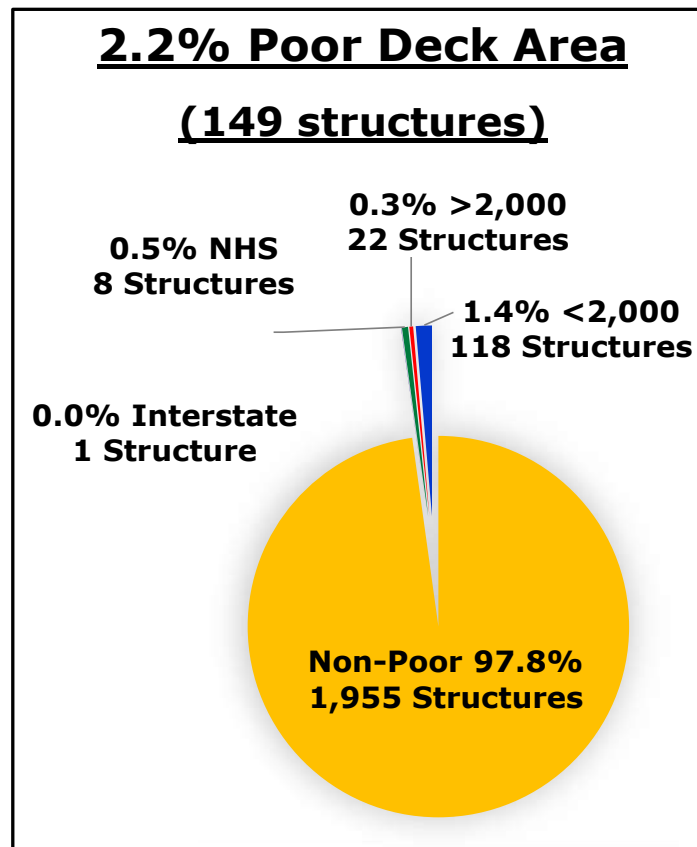
District 9 Assets Bridge & Roadway

Bridge Assets

Statewide



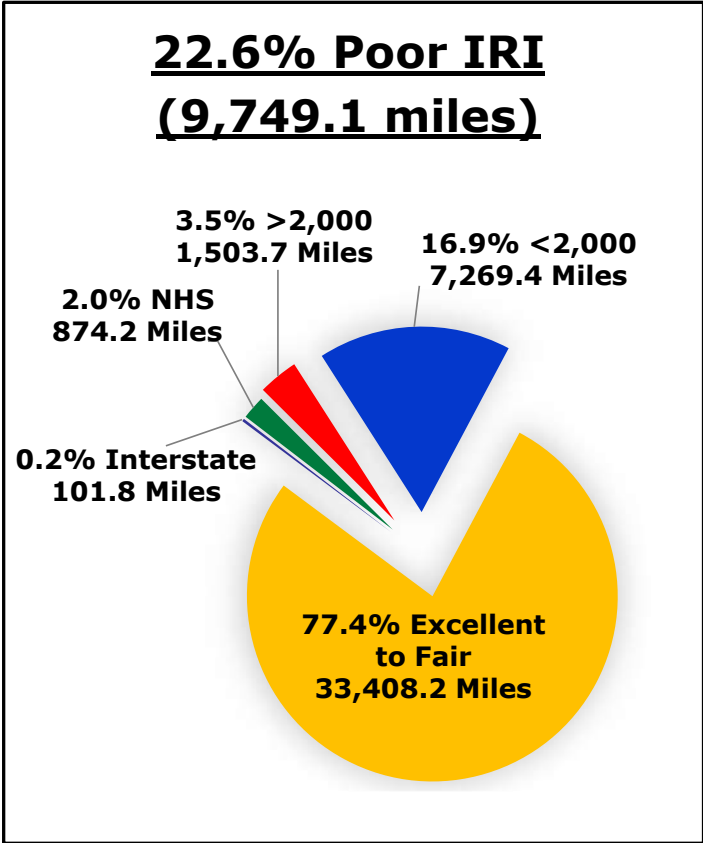
District 9



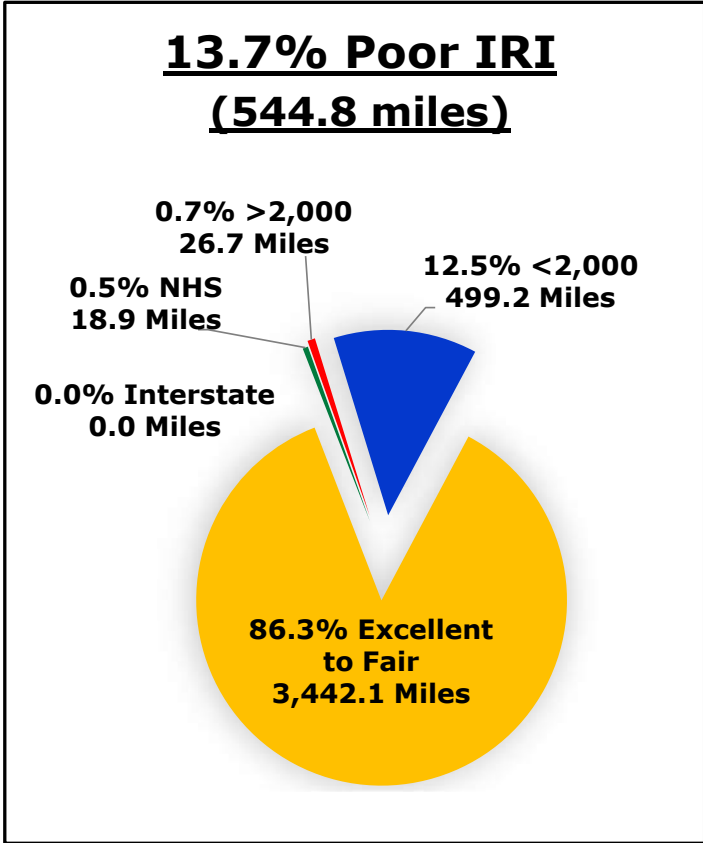
Data from 2020 Annual Performance Measures Reports

Roadway Assets

Statewide



District 9

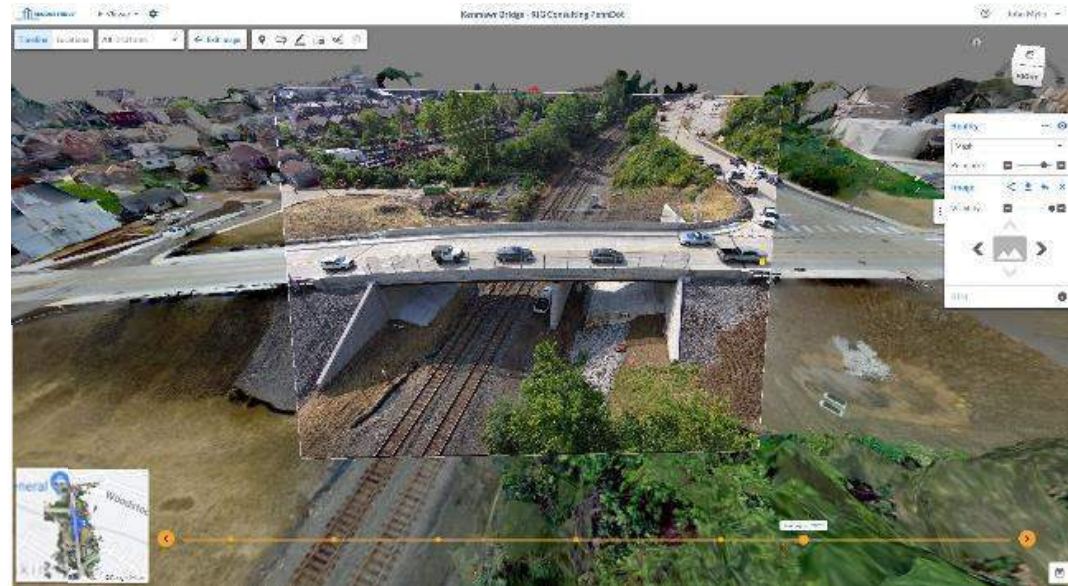


Data from 2020 Annual Performance Measures Reports

INNOVATION

MOVING INTO A VIRTUAL WORLD

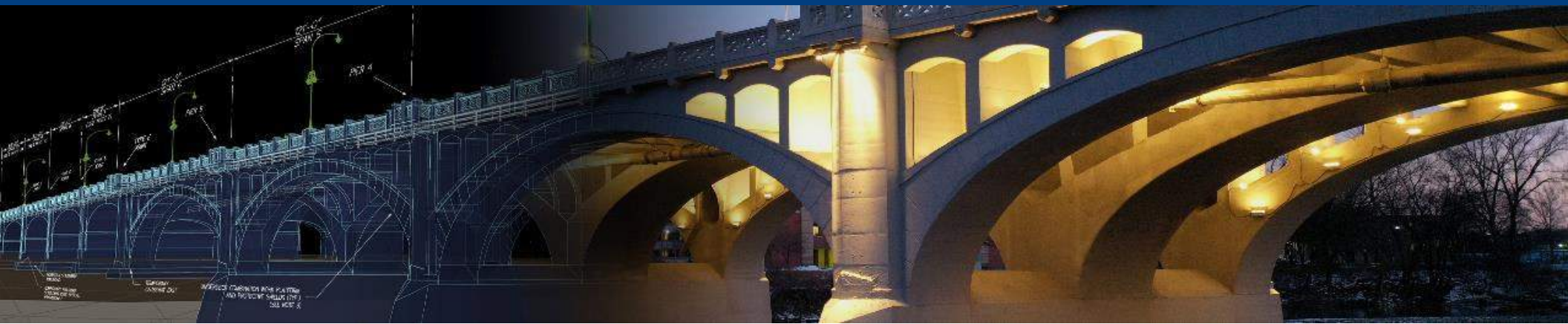
Augmented Reality & Virtual Inspections



Unmanned Aircraft Systems



DIGITAL DELIVERY DIRECTIVE 2025 (3D2025)



Vision: By 2025, construction projects will be bid using 3D technology and no longer be in a traditional construction plan format.

- Phase II (Development) of the Directive
- Pilot Projects
 - Digital As-Builts/Single Project PDF/Roadway & Bridge Modeling
- Stakeholder Coordination



FUNDING

BIL – BIPARTISAN INFRASTRUCTURE LEGISLATION

“New” Federal Funds

Year	Current Funding	Highway Funds	Special Bridge	Total Add'l Funds
2022	\$1.8 Billion	\$377 M	\$262 M	\$639 M
2023	\$1.8 Billion	\$421 M	\$294 M	\$715 M
2024	\$1.8 Billion	\$465 M	\$327 M	\$792 M
2025	\$1.8 Billion	\$510 M	\$360 M	\$870 M
2026	\$1.8 Billion	\$557 M	\$392 M	\$949 M
				\$3.965 B

*\$1.0 Billion in matching State Dollars

*Must also fund pre-construction phases



2022 FUNDING: BIL

FFY 2022 IJA Additional Funding (\$000's)	
Blair MPO	\$4,343
Cambria MPO	\$5,920
S Alleghenies RPO	\$12,838
District 9	\$24,101



2023 – 2026 FUNDING: BIL

	Original Guidance FFY 2023-2026 (\$000's)	IJA Estimates FFY 2023-2026 (\$000's)	Increase (\$000's)	Increase (%)
Blair MPO	\$44,750	\$66,222	\$21,472	47.98%
Cambria MPO	\$65,669	\$95,208	\$29,539	44.98%
S Alleghenies RPO	\$134,284	\$197,748	\$63,464	47.26%
District 9	\$244,703	\$359,178	\$114,475	46.78%



2023 TIP Financial Guidance

Total FFY 2023-2026 Highway/Bridge Base Funding Allocation (\$000)

	Flexible			Roads			Bridges			Total		
	Original	IIJA	Difference	Original	IIJA	Difference	Original	IIJA	Difference	Original	IIJA	Difference
Blair MPO	\$ 15,166	\$ 21,140	\$ 5,974	\$ 14,450	\$ 15,475	\$ 1,025	\$ 15,134	\$ 29,607	\$ 14,473	\$ 44,750	\$ 66,222	\$ 21,472
CCMPO	\$ 23,035	\$ 35,113	\$ 12,078	\$ 25,302	\$ 26,542	\$ 1,240	\$ 17,332	\$ 33,553	\$ 16,221	\$ 65,669	\$ 95,208	\$ 29,539
S Alleghenies	\$ 45,707	\$ 62,862	\$ 17,155	\$ 43,702	\$ 45,150	\$ 1,448	\$ 44,875	\$ 89,736	\$ 44,861	\$ 134,284	\$ 197,748	\$ 63,464
District 9-0	\$ 83,908	\$ 119,115	\$ 35,207	\$ 83,454	\$ 87,167	\$ 3,713	\$ 77,341	\$ 152,896	\$ 75,555	\$ 244,703	\$ 359,178	\$ 114,475

- Approximately 68% of IIJA funding is directed toward bridges
- Approximately 51% of IIJA bridge funding is directed toward bridges off the federal aid system
 - Local bridges
 - Low traveled state roads
- IIJA funding: \$23M Bridges on low traveled roadways vs. \$31M on High priority Routes (NHS)

PROGRAM DISTRIBUTION

	Cost Based Percentage						
	2016	2017	2018	2019	2020	2021	2022*
Highway	53%	64%	60%	70%	61%	71%	72%
Bridge	35%	21%	28%	19%	24%	17%	20%
Other	12%	15%	12%	11%	15%	12%	8%

*2022 Projected



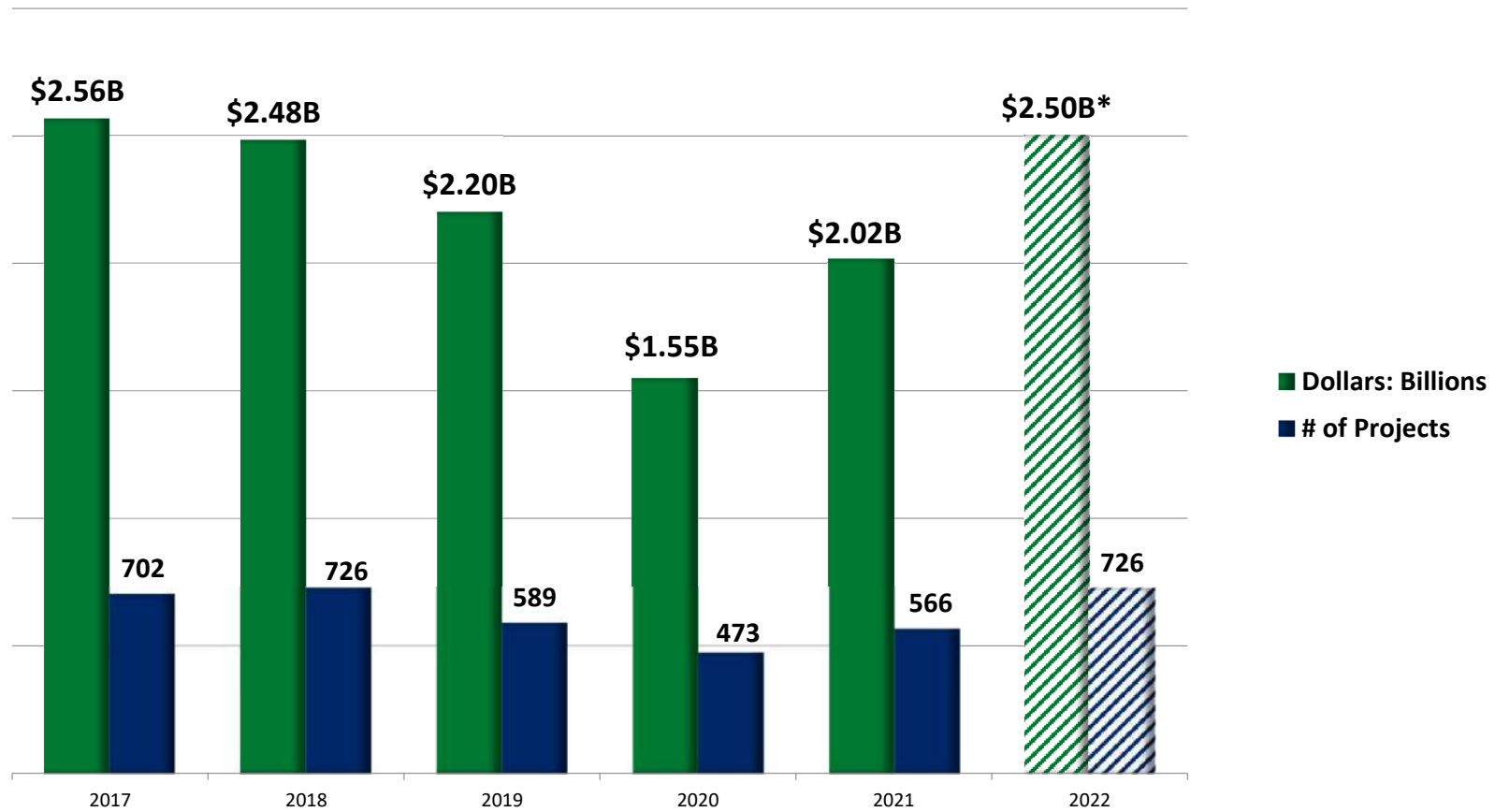
DISTRICT 9 2023 TIP SUMMARY

	(\$000s)	#	(\$000s)	#	% of Funding Total
	Original (December 2021)		Revised (January 2022)		
Base Financial Guidance Amount	\$ 244,705	171	\$ 359,178	238	
Roadway Total	\$ 157,490	69	\$ 203,302	80	56.6%
Raised Pavement Markings	\$ 800	6	\$ 800	6	0.2%
Safety and Congestion	\$ 27,584	11	\$ 31,691	11	8.8%
Highway Restoration	\$ 129,106	52	\$ 170,061	60	47.3%
TSMO (50% match)	\$ -	0	\$ 750	3	0.2%
Bridge Total	\$ 87,215	102	\$ 155,876	158	43.4%
Bridge Preservation Line Item	\$ 5,077	3	\$ 5,176	3	1.4%
State Bridges	\$ 59,445	68	\$ 118,092	119	32.9%
Local Bridges	\$ 22,693	31	\$ 32,608	36	9.1%



PROGRAM

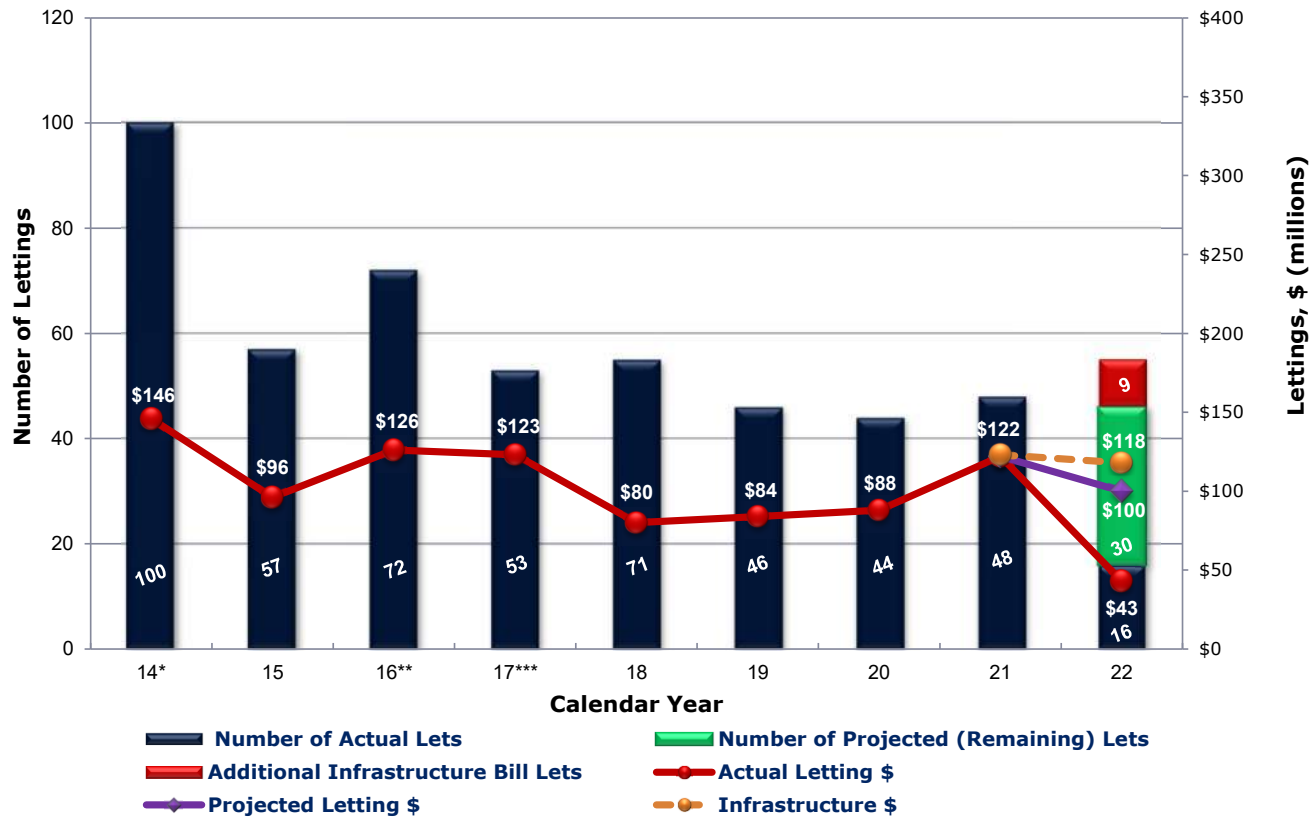
PROJECT LETTING TRENDS



2022 Projected *



DISTRICT 9-0 PROJECT LETTINGS



* 2014 totals do not include US 219 Somerset to Meyersdale Bridges Contract (\$67M)
 ** 2016 totals do not include US 219 Somerset to Meyersdale Paving Contract (\$52M)
 *** 2017 totals do not include US 219 ITS Contract (\$1.6M): Includes I-99 (\$27.8M)



2020-2022 CY Lettings District 9-0

	2020	2021	2022
Construction Estimate	\$88 million	\$122 million	\$118 million
Project Count	44	48	55
Miles Resurfaced	73	184	150
Number of Bridges	51	45	45
Safety Projects	8	5	3

CONCERNS

- **Total Projects let to Date: 26**
 - Contractors Total Bid to Date: \$75,750,246
 - Design Estimate Total to Date: \$69,171,680
 - Difference to Date: 9.5%





2021 End of Season Meeting Follow-Up Items

Follow-Ups from 2021 End of Season Meeting – still under review

Industry does not feel the special provision for screening of millings to be retained by the Department or, alternately, to provide washed #8s is good for the Department from a cost perspective

Utility delay concerns

Department staff workload and increasing documentation requirements (for both Department and contractor staff) ~ impacts to prompt payments, timely field decisions

Tight projects schedules (permit restrictions, school restrictions, etc.)

Industry recommended that when the Department assigns inspection staff to projects, they attempt to distribute staff evenly to pair staff who are good in the field with staff whose strength is on the documentation end

Industry requested the District consider implementing a tool for contractors to evaluate IICs

Due to on-going supply chain issues, will the Department be expanding the list of stored materials?

Example Follow-Ups from 2021 End of Season Meeting

Concerns were raised about tight projects schedules

- **Industry looking more for flexibility on calendar timeframes/ windows, not necessarily durations**
- **Limit night work requirements due to staff shortages and associated costs**

❖ *2021 Breakdown:*

48 Contracts Let

11 Contracts had some form of compression (+8hr day, +5 day work week, etc)

11 Contracts had night work

24 Contracts had RULD, A-Bx, etc

❖ *2022 Breakdown:*

55 Contracts Let

4 Contracts have some form of compression (+8hr day, +5 day work week, etc)

5 Contracts had night work

21 Contracts had RULD, A-Bx, etc

Example Follow-Ups from 2021 End of Season Meeting

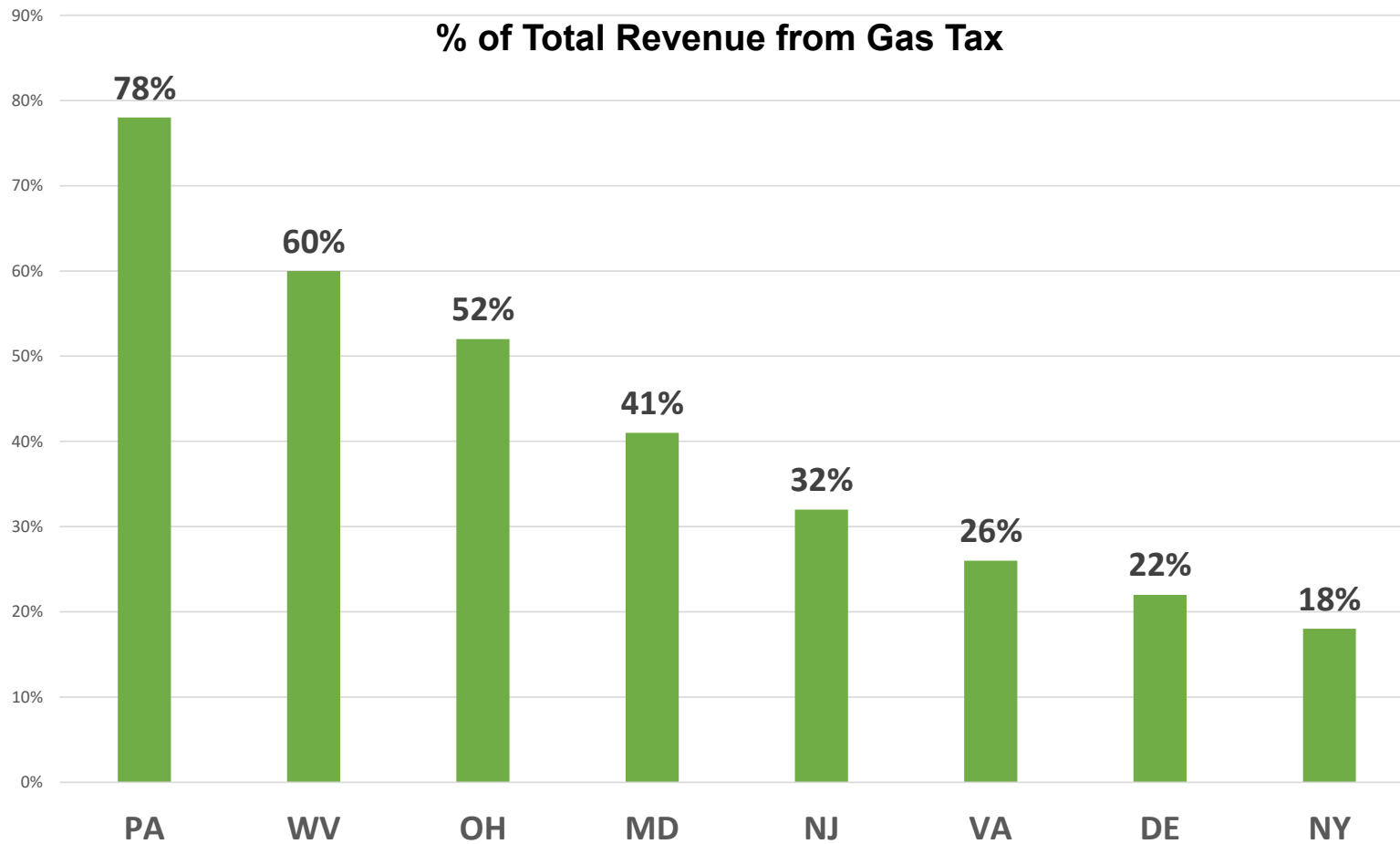
Improvements are needed in answering ECMS questions during bidding, particularly 'bid as is' type answers

- ❖ *District reviewed all ECMS bid questions answered from 12/1/20 to 11/30/21 (268 total)*
- ❖ *18 responses (6.7%) included verbiage such as the following:*
 - *Bid per plan*
 - *Bid as indicated*
 - *Bid in accordance with contract documents*
- ❖ *District opinion is that 9 of these responses were appropriate in the context of the question asked (5 also contained additional clarification explaining the 'bid as is' response)*
- ❖ *District does agree that 9 could have contained additional detail/ clarification to better explain the 'bid as is' answer*
- ❖ *These questions & responses have been reviewed with District Project Managers to stress that 'bid as is' type answers need to be minimized but, if such response is appropriate, any additional clarification explaining the response should be included*

TRANSPORTATION REVENUE OPTIONS COMMISSION

PA





BE SAFE ON OUR ROADWAYS!



Be Safe PA.
Speed limits protect everyone.
Slow Down, Save a Life

SPEED LIMIT 35

 www.PennDOT.gov/Safety





Be Safe PA.

Impaired driving crashes are 100% preventable.
Don't Drive Impaired

Best Dad Ever



Be Safe PA.

In a moment like this, seat belts save lives.

Don't Drive Unbuckled





Be Safe PA.

Distractions are everywhere.
Don't Drive Distracted

Thank You – Questions?

