



District 9 Process






Thomas A. Prestash, P.E.
District Executive
April 10, 2017



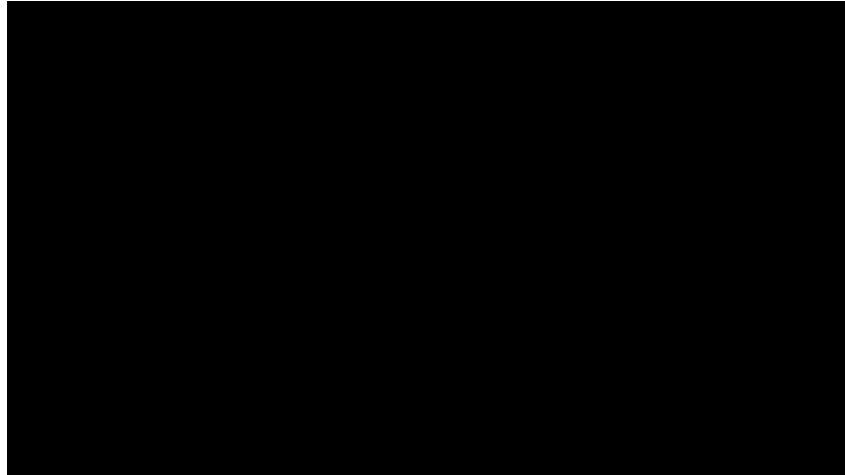
Planning in a 21st Century State DOT



- “State DOTs are no longer just held accountable for the transportation system; they are also held accountable for how the system supports and improves quality of life for communities.”



Secretary Richards



PennDOT Connects

- Identify needs of communities and related issues early in project planning
- Consider community transportation needs
 - ✓ Bicycle and pedestrian accommodations
 - ✓ Transit access
 - ✓ Stormwater management
 - ✓ Green infrastructure



PennDOT Connects

- Community transportation needs assessment based on:
 - ✓ Comprehensive plans
 - ✓ Other local planning studies
- Local government must demonstrate features are a (documented) element of the community vision
- Cost is a consideration, but not reason enough to disregard improvements



Collaboration Requirements

- Collaboration is required for developing the scope of a project
 - Planning Partner
 - Planning Commission
 - All municipalities within the planned limits
 - Other key stakeholders:
 - Transit
 - Bike groups, etc
 - Program Center
 - Public
- Collaboration must be documented



Implementation Timeframe

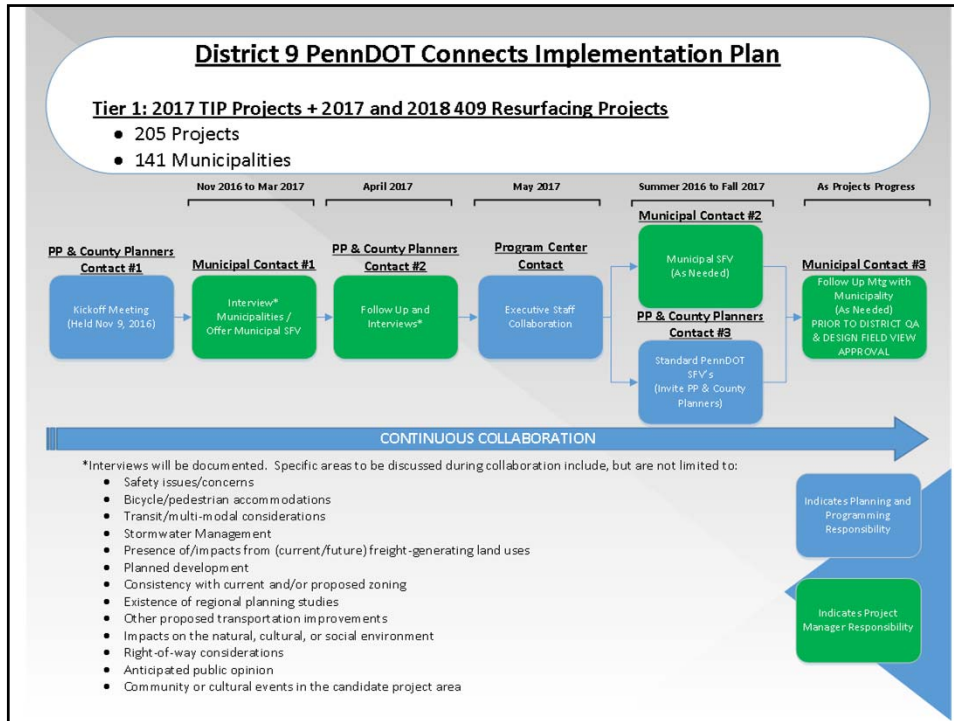
- **Tier 1: 2017 TIP & 409 Projects**
 - 205 Projects
 - 141 Municipalities
 - MPO approved project need as part of 2017 TIP development
 - Interviews complete by March 2017
 - If project spans more than one municipality, multiple interview forms are generated
- **Planning Partner/County Planner Meeting (Apr 2017)**
 - Invite Program Center
 - Interview summary of all projects in county
 - Summarize follow-ups/major issues
 - Project specific discussion as needed
 - Obtain Planning Partner approval
- **Submit Package for Program Center Approval (Apr 2017)**



PennDOT Connects – Progress to date

| County | Projects | Interviews Complete | % Complete |
|----------------|----------|---------------------|------------|
| Bedford | 32 | 30 | 94% |
| Blair | 32 | 32 | 100% |
| Cambria | 46 | 34 | 74% |
| Fulton | 11 | 10 | 91% |
| Huntingdon | 33 | 26 | 79% |
| Somerset | 51 | 39 | 76% |
| District Total | 205 | 171 | 83% |





Documentation Requirements

- Pre-Meeting Correspondence:
 - Brief explanation of process
 - Project Summary Form
 - E-mail with additional items to be discussed to help municipality get prepared
 - Invite Program Center, MPO/County Planner

Project Summary Form Date: 11-10-2016

Project Name: Franklin Street Bridge Rehabilitation

County: Cambria Municipality(ies): City of Johnstown

State Route: 3055 Road Name: Franklin Street

Location: On Franklin Street (SR 3055) over Stonycreek River between Vine Street (SR 3026) and Somerset Street

Description: On Franklin Street (SR 3055) over Stonycreek River between Vine Street (SR 3026) and Somerset Street

Anticipated Construction Year: 2020

Project Purpose: The purpose of the project is to address deficiencies of the bridge

Project Need: Correct deficiencies to the bridge

Project Scope: The project will rehabilitate the bridge

Location Map

Documentation Requirements

- Meeting:
 - Explain PennDOT Connects
 - Explain scope of the project (Tier 1)
 - Conduct interviews
 - Using Project Initiation Form
- Post Meeting:
 - Finalize interview forms
 - Complete follow-ups as needed

Community Collaboration Program Meeting Documentation
PennDOT District 9-0

Meeting Date: Dec 20, 2016
 Meeting Location: Johnstown City Hall
 Meeting Time: 1:00pm
 Attendees: Vince Greenland, Jim Frus, Dave Lybarger

Attendees: See attached sign in sheet for e-mail addresses.

Project Name: Franklin St Br Rehab MPMS #: 94479
 State Route: 3055 Segment/Offset: BMS ID: 11 3055 0020 0036
 County: Cambria Municipality(ies): City of Johnstown
 Project Location: On Franklin Street (SR 3055) over Stonycreek River between Vine Street (SR 3016) and Somerset Street
 Project Length: 258' long bridge Anticipated Construction Year: 2020

Existing Pavement Width: Existing Shoulder Width: Existing Bridge Width: 41'
 Proposed Pavement Width: Proposed Shoulder Width: Proposed Bridge Width:
 Existing ROW:
 Crash History:
 Proposed Traffic Control: BMS has 1 mile detour.

Project Purpose: The purpose of the project is to address the structural deficiencies of the bridge

Project Need: Correct deficiencies of the bridge

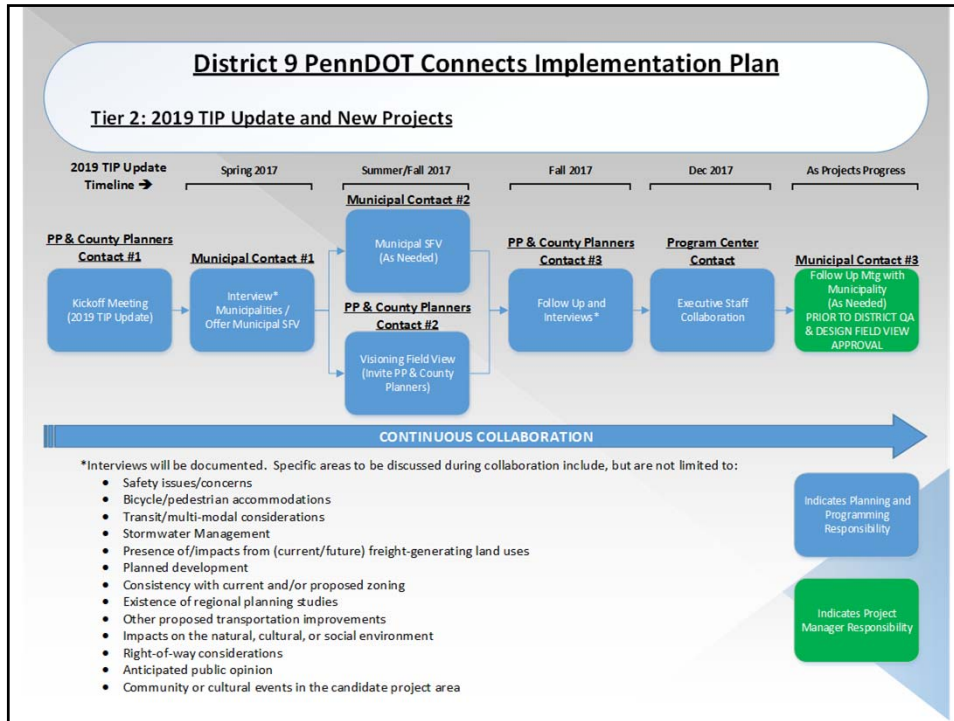
The project will rehabilitate the bridge consisting of superstructure repairs & paint (color will be coordinated with the city)
 Maintain existing width
 Built in 1900
 ADT 6,112
 25 mph

Short Project Description and Scope:

Ongoing Documentation

Developed a project tracking spreadsheet
 – Tabular list of results from interviews, including follow up items

| County | Municipality | MPMS | SR | Project Name | Let Date | Meeting Date | Additional Items | Remarks |
|--------|---------------------|-------|-------------------------|---------------------------|------------|--------------|--|--|
| Blair | TYRONE BOROUGH | 21541 | 12012 SR 4022 to SR 453 | | 2/9/2017 | 2/9/2017 | | |
| Blair | GREENFIELD TOWNSHIP | 21908 | 7207 | T-310 Friesville Bridge | 12/9/2016 | 12/9/2016 | | p. 2 Pedestrians - pedestrians currently crossing bridge to access park and schoolbus pickup. Consider sidewalk or wider shoulder |
| Blair | ALTOONA CITY | 22088 | 4015 | Wapshamock Avenue Spring | 2/10/2012 | 12/8/2016 | Follow-up with Mark Criste on drainage issues | |
| Blair | ALEXANDER TOWNSHIP | 74400 | 4002 | Inger Run Bridge/Seg 110 | 12/17/2010 | 2/7/2016 | | |
| Blair | LOGAN TOWNSHIP | 74403 | 4013 | North Juniata Homers Gap | 2/10/2012 | 12/1/2016 | | |
| Blair | LOGAN TOWNSHIP | 91533 | 99 | 1-99 17th St Grazienville | 3/18/2017 | 12/9/2016 | | |
| Blair | ALTOONA CITY | 92595 | 4008 | Horseshoe Crv - Lgn Blvd | 5/4/2017 | 12/8/2016 | | p. 2 Pedestrians - pedestrians us the Blvd - not much on Broad & Beebe p. 3 Bicyclists - bike lane toward horseshoe - not in the near future though p. 7 Stormwater - can the city put a BMP in our island? Would need agreement & HOB. No depressed curb at driveways so 3 or 4 of them placed a bituminous ramps in the gutter line to get access to driveway - needs corrected p. 8 Safety - yellow hatching at island w/skip marks thru intersection (in the city). Broad Ave - stop except Rt. turn at 40th St. Traffic on city lit (COPI) doesn't know Broad Ave. Has stop except lit turn. p. 9 Bicyclists - heavily used by bicycles, popular road for fund raisers B&K runs, p. 5 Congestion - 58th St. intersection sight distance p. 7 Stormwater - inlet at 58th St. has water bypassing it p. 8 Safety - accidents at 58th St potential sight distance issue looking left on 58th St to go onto SR4008 |
| Blair | LOGAN TOWNSHIP | 92595 | 4008 | Horseshoe Crv - Lgn Blvd | 5/4/2017 | 12/1/2016 | | |
| Blair | SNYDER TOWNSHIP | 92648 | 4027 | Hutchinson Run Bridge | 10/8/2020 | 1/30/2017 | Township noted there are businesses close to the bridge that will likely be affected during construction. No impacts expected to any Borough sponsored community events. Wes Burket mentioned MS 150 Bike ride may go through the project and coordination would need to be made with the Multiple Sclerosis Society to determine any impacts | |
| Blair | SNYDER TOWNSHIP | 92648 | 4027 | Hutchinson Run Bridge | 10/8/2020 | 2/3/2017 | | |
| Blair | ALTOONA CITY | 92663 | 784 | PK384 Culvert Replacement | 1/20/2012 | 12/8/2016 | | |
| Blair | TYRONE BOROUGH | 92665 | 4029 | Clay Ave Sinking Run Br | 10/8/2020 | 2/3/2017 | If Clay Ave and Hutchinson Run are to be constructed in same year borough prefers Clay Ave to be built first to ease impacts on school. Borough recently replaced storm sewer along Clay Ave. | p. 2 Pedestrians - Sidewalks start on right side at the bridge. On the left side of the bridge it starts at the school and goes across the bridge to SR 453. Don't specifically say one way or another if they would sign a maintenance agreement, but they have signed them in the past. p. 3 Bicyclists - Borough stated bicycles use Clay Ave due to the width of the road. The "Turkey trail" is in the area that locals walk and ride bikes on. p. 5 Congestion - No signals on road. Borough would want signals at Clay Ave and 15th St intersection because drivers don't see stop signs and recently there was an accident w/ significant injury. Borough willing to take over maintenance. School traffic plus truck traffic from industries in the area. p. 6 Freight - Truck traffic uses Clay Ave as a route to and from the industrial park. Borough is not aware of any new facilities. |



Future Direction

2019 TIP Update

- **GOAL: Program Center Collaboration Approval by Dec 2017**
 - Spring 2017 - Municipal Interviews
 - Summer/Fall 2017 – Visioning Field Views
 - Fall 2017 – Planning Partner Approval

Ongoing Collaboration

- Key Points of Collaboration
 - Initial Meeting
 - Visioning Field View
 - Scoping Field View
 - Follow-Ups on outstanding issues
 - Throughout the project development process as needed

- Additional Collaboration Opportunities
 - Planning Partner Meetings
 - Annual Outreach Meetings
 - Project specific meetings
 - Public Officials/Plans Display
 - Transit Agencies – Met with AmTran
 - Local bike groups and other stakeholders

PennDOT Connects – Consultant Projects

- Prior to consultant selection
 - Consultant (in preparation of the SOI)
 - Consider project needs/potential challenges
 - ROW, utilities, permits, traffic control, etc.
 - Consider community needs
 - Bike/Ped, congestion, events, land use, etc.

 - District 9
 - Engage the municipality, planning partner, county and other stakeholders



PennDOT Connects – Consultant Projects

- After Notice To Proceed
 - Consultant (in preparation of the Line, Grade and Typical Section Submission)
 - Provide recommendations to the District regarding each of the follow-up items for the project
 - May include (if warranted):
 - » Alternatives, cost estimates, environmental & ROW impacts, etc.
 - District 9
 - Review recommendations & provide direction for what to incorporate into DFV submission
 - All recommendations will be subject to the District's escalation process

PennDOT Connects – Consultant Projects

- District Escalation Process
 - All requests that differ from the original need must be discussed and approved by Senior Project Manager
 - Any >\$10,000 additional cost; ROW or environmental impacts also require Portfolio Manager approval
- DFV is the final checkpoint in project development process to ensure follow-ups occur
 - Follow-ups must be documented and submitted to Senior Project Manager
 - Cost-Benefit Analysis
 - Design Field View is not approved until follow-ups are complete



QUESTIONS?

